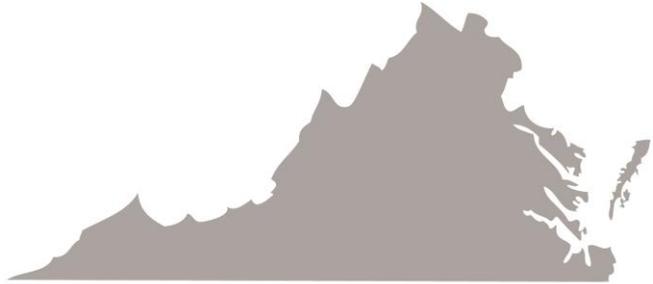


The logo for VTRANS, with 'V' in a large, bold, blue font and 'TRANS' in a smaller, bold, blue font.

VIRGINIA'S
TRANSPORTATION PLAN

Mid Term Needs
Assessment Regional
Workshop Summary
Lynchburg Area
Wednesday August 21, 2019



Office of Intermodal Planning and Investment

1221 E. Broad Street

Richmond, Virginia 23219

FINAL September 23, 2019

Prepared for OIPI in support of VTrans, Virginia's Statewide
Multimodal Transportation Plan

Contract Number 47082, Task Two: Agency Involvement

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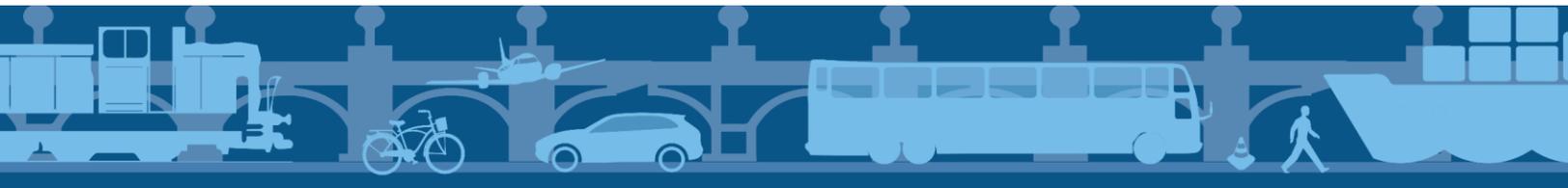
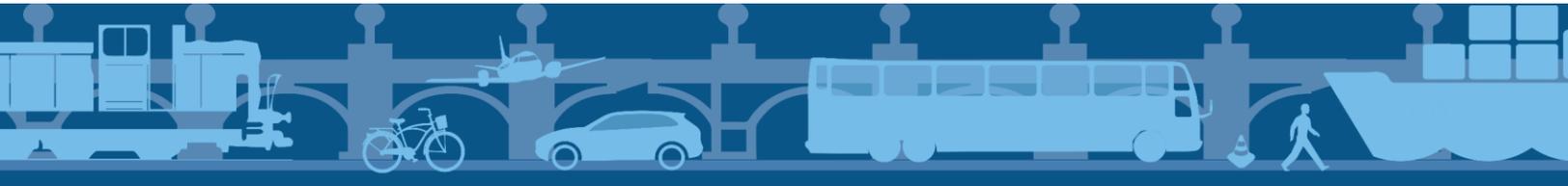


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1 INTRODUCTION

This report summarizes the input from a workshop conducted by the Virginia Office of Intermodal Planning and Investment (OIPI) with representatives of local, regional, and state agencies that support transportation planning for the Lynchburg area. The purpose of the meeting was to elicit input on the analysis methods (specifically, key performance measures) and the regional results of initial analyses conducted to identify statewide transportation needs for the coming seven to ten years.



2 MEETING LOCATION AND PARTICIPANTS

The workshop was conducted at The Kirkley in Lynchburg, Virginia, from 10:00 a.m. to 2:00 p.m. Table 1 provides a list of participants.

Table 1: Workshop Participants and Invited Representatives

Name	Agency	Title
Regional and Local Representatives		
Austin Mitchell	Campbell County	Zoning and Subdivision Administrator
Daniel Witt	Charlotte County	County Administrator
Gary Christie	Central Virginia PDC	Executive Director
Gerald Spates	Town of Farmville	Town Manager
Jeremy Bryant	Amherst County	Director of Community Development
Kelly T. Hitchcock	Central Virginia PDC	Planning & Development Director, RideSolutions Coordinator

Lee Newland	City of Lynchburg	City Engineer
Mary Zirkle	Town of Bedford	Economic Development Coordinator
Natalie Wilkins	Greater Lynchburg Transit Company	Marketing and Customer Experience Manager
Sarah Elam Puckett	Prince Edward County	Assistant County Administrator
Scott Smith	CVPDC/CVMPO	Transportation Planning Director
Todd Carroll	Liberty University	Corporate Civil Engineer
Todd Fortune	Commonwealth Regional Council	Deputy Director
Nicci Edmonston	Buckingham County	Planning/Zoning Administrator
Glenn Millican	Lunenburg County	Planning and Economic Development
Philipp Gabathuler	CVPDC	Senior Planner
Johnnie Roark	Appomattox County	Director of Community Development

Additional Regional and Local Representatives Invited but Unable to Attend

Mariel Fowler & Gregg Zody	Bedford County
JP Duncan & Vivian Giles	Cumberland County
Mark Courtney	Lynchburg Regional Airport
Stephen Carter & Candy McGarry	Nelson County
Waverly Coggsdale	Town of Altavista
Sara Carter	Town of Amherst
Gary Shanaberger	Town of Appomattox
Russell Thurston	Town of Brookneal
Karen Price	Town of Charlotte Courthouse

Linda Paige	Town of Dillwyn
Mary Sands	Town of Drakes Branch
Robyn Fowler	Town of Kenbridge
Steven Morris	Town of Keysville
Brenda Driskill	Town of Phenix
Rodney Newton	Town of Victoria

State Agency Staff

Daniel Sonenklar	DRPT	Statewide Transit Planner
Emily Stock	DRPT	Manager of Rail Planning
Katherine Graham	VDOT Central Office	Multimodal Planner
Katherine Molin	DRPT	Mobility Programs Manager
Rick Youngblood	VDOT Lynchburg District	Planning Manager
Stephen Smiley	VA Department of Aviation	Senior Aviation Planner
J.P. Morris	VDOT Bedford Residency	Assistant Resident Engineer - Land Use
Brian Casella	VDOT Bedford Residency	Assistant Resident Engineer - Land Use
Carol Linkenhoker	VDOT Salem District	Planning Specialist
David Cook	VDOT Lynchburg District	Transportation Planner
Jitender Ramchandani	OIPI	Transportation Planning Program Manager
Chris Wichman	OIPI	Transportation Planner
Katie Schwing	OIPI	Transportation Planner

Consultant Facilitators and Scribes

Walt Cole	EPR, P.C.	Facilitator
Will Cockrell	EPR, P.C.	Facilitator

Jessica Dimmick	EPR, P.C.	Facilitator
Michael Stafford	CDM Smith	Scribe
Marissa Sperry	EPR, P.C.	Scribe
Thomas Jackson	EPR, P.C.	Scribe

3 AGENDA AND MATERIALS

Following a plenary presentation and discussion of the VTrans needs assessment method and performance measures, the participants broke into small groups to review the information developed for the region. They regrouped at the end of the meeting to share their findings and to hear about the process and schedule for developing, reviewing, and finalizing the VTrans mid-term needs assessment.



Upon sign-in, each participant received a packet with the following materials, all of which are available for download from VTrans website.¹

- Agenda
- Plenary presentation slides
- VTrans Summer 2019 Newsletter
- VTrans Mid-Term Needs Frequently Asked Questions (FAQ)
- Comment Form
- Regional maps, charts, and/or tables of data developed for the region. Detailed descriptions of each measure and analysis method are included in the plenary presentation slides posted to the VTrans website.

4 SYNTHESIS OF COMMENTS

The following section provides a summary of comments about each performance measure, compiled from the plenary session, breakout groups, and comment sheets. The appendix includes transcripts of the sessions and sheets, including photos of the marked-up maps developed by each breakout group. After the participants have reviewed and vetted the draft report, OIPI will synthesize the comments that were associated with the maps and upload them to the online InteractVTrans map.² In addition to serving as a repository for regional workshop comments, InteractVTrans provides a publicly available resource for ongoing input from local stakeholders and the public.

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¹ VTrans website: www.vtrans.org Location of workshop summaries: <http://vtrans.org/get-involved/online-meetings/VTrans-Mid-Term-Needs-Regional-Workshops>

² InteractVTrans: www.vtrans.org/mid-term-planning/InteractVTrans

OIPI will consider each comment during the process of refining the needs assessment methodology and developing the draft needs, and will respond directly to specific questions posed by stakeholder. As noted in the plenary presentation, OIPI will present the initial list of needs to the Commonwealth Transportation Board in October 2019, and the final needs assessment with a request for Board action in December 2019.

Table 2 Synthesis of Comments

	Comment
Congestion: Percent Person Miles Traveled in Excessively Congested Conditions (PECC)	
1.	US 460 and US 501 are congested limited access highways, but they are not shown on the map. This map and this definition of congestion do not reflect congested conditions in Lynchburg.
2.	US 460 westbound between Candler's Mountain Road and Greenview Drive is particularly problematic. Traffic on the Candler's Mountain Rd off ramp regularly backs up onto 460 Westbound.
3.	In Map 1-CU, the part of the US 29 Bypass crossing over the river shown in red does not make sense - this segment is not congested. It may show up as red because traffic must slow down to get onto the ramp. Another reason might be because there was a construction project in 2018.
4.	US 29 Business at Madison Heights is a top congestion issue in Amherst County. The congestion shown on US 29 Business on Map 1-C is accurate and could be extended further north.
Congestion: Travel Time Index (TTI)	
5.	Map 2 is showing congestion on US 460 and Village Hwy (VA-24) between Appomattox and Rustburg, which was surprising to 2 of the 3 groups. Please double check this. There was roundabout construction that could have caused the map to light up. The high school may also have been a contributing factor. One group confirmed that this section of US 460 is consistently slow going into Lynchburg in the mornings and leaving Lynchburg in the evenings.
6.	Only a portion of Timberlake Rd (US 460 Business) is showing up on the map. Participants expected more of it would show up as congested.
7.	There is a congestion issue at the intersection of US 221 and US 501, but this area is not showing up on the map. US 221 is congested from US 501 to Route 663.
8.	US 29 Business to Lakeview Drive is congested during peak hours but isn't showing up on the map.
9.	The area of Laxton Road to Timberlake Road to Greenview Drive is a congested area, but it is not shown on the congestion maps.

	Comment
10.	It would be helpful to have the methodology for the TTI measure. Reliability is relative to other areas.
11.	The reliability maps show US 29 Bypass through Amherst County and in the southern part of Campbell County as unreliable, but it is one of the most reliable roads in the region. When accidents do occur, it can take several hours to clear up and there is nowhere to get off. These are rare occurrences.
Reliability: Buffer Time Index (BTI)	
12.	At the intersection of US 221 and US 501, US 501 northbound is not showing up, but it has reliability issues, just like the southbound and the other roads approaching this intersection.
13.	Please add highway on- and off-ramps to the map for this measure.
14.	The BTI map shows portions of the Blue Ridge Parkway as unreliable. The concept of reliability is not applicable to the Blue Ridge Parkway.
15.	Map 4 shows Campbell County Route 24 as unreliable, but there are no reliability issues here.
16.	Map 4 shows the intersection of US 460 at Route 24 (at the Appomattox and Campbell County border) as unreliable, but there are no reliability issues here.
17.	The BTI map shows Old US 460 near Pamplin in Prince Edward County as blue. Participants did not understand why this was showing up.
18.	Route 20 in northern Buckingham County is shown in blue. There was a construction project here in 2018.
19.	There is a blue dot west of Farmville. Please review this dot.

Passenger Rail: Amtrak Station On-Time Performance

20.	The business community would like to see earlier train arrival times to D.C. and improved air service from Lynchburg to Dulles.
21.	Intermodal freight is a large economic development opportunity. The Buckingham Branch railroad company is considering developing an intermodal center at the Heartland Industrial Park in Charlotte County, which would take trucks off the roads. The Town of Crewe is an intermodal truck point.
22.	Please show short line rail infrastructure on the map.

Comment

Accessibility: Transit Access Deficit to Activity Centers

23.	<p>Multiple groups indicated the activity centers are hard to understand. Participants questioned how the activity centers were defined and how they would be used. Many of the activity centers on the map did not accurately reflect the true centers of activity, especially those within the City of Lynchburg. For example, the AC named “Walmart Supercenter” while other similar stores outside of Lynchburg that are not showing up. Localities have Points of Interest layers that could be used to identify the activity centers.</p> <p>A participant that submitted written comments indicated Activity Centers should be defined by the number of employees, the needs of those employees, and other factors. He/she also said Centers outside of MPOs need to be defined too.</p>
24.	Big Island is not a local serving activity center anymore. It is more of a freight-dependent activity center.
25.	Brookneal is missing from the map and should be added as an activity center.
26.	Farmville may need to be included. It is an isolated activity center.
27.	The Dearington activity center point should be moved to the hospital and health centers. The hospital is a major employer in the area.
28.	The map shows a medium transit deficit at Liberty University, but there is no transit deficit within the campus nor in the immediate surrounding area. There is a difference between accessibility <u>within</u> an activity center and accessibility <u>to</u> an activity center.
29.	The Forest activity center is an industrial park.
30.	The Lynchpin Industrial Park has a lot of traffic and train service and should be added.
31.	Should MPOs identify the activity centers if OIPI provides criteria? The Central Virginia MPO offered to do so if actual placement of the points matters

Travel Options: Disadvantaged Population Beyond ¼ Mile Access to Fixed Route Transit

32.	<p>The maps seem a little odd or misleading at the local level. The maps appear to be looking at the central geographic location of census tracts, not population centers. Some of the areas shown as disadvantaged actually have transit. Please clarify what the limitations of the data are. The Central Virginia PDC offered to provide more recent data on transit routes and stops.</p>
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	Comment
33.	In Amherst and Bedford counties. There are trailer parks and people who are disadvantaged living next door to high-income households. Some areas are National Forest with no people living there - look into data for disadvantaged population in areas that have no residents.
34.	It would be helpful to show park-and-ride locations and other types of service (e.g. van-sharing) on these maps.
35.	GLTC Route #5 goes into the Old Town Madison Heights area, but there is limited transit service here. This area is not shown on the maps as disadvantaged, but it is, and it should be added. It is a geographically challenging (hilly) area with an urbanized pocket, and it has the highest percentage of low-income residents and high unemployment.
36.	Altavista and the Town of Amherst need transit connector service to Lynchburg. Please make sure the Town of Amherst is an activity center on the maps.
37.	How will VTrans address areas that are “not viable” for fixed route service? Do these areas have zero transit service needs?
38.	Appomattox County and Bedford County do not have any transit service, but the map is showing fixed route service here. Why is it shown?
39.	The area around Dot 20 (Group 2) is not disadvantaged, but it is shown in Map 7. Look into data for this area.
40.	The area around Dot 21 (Group 2) is one of the most low-income areas in Amherst County, but it is not shown in blue. It should be shown.
41.	Farmville is served by multiple transit systems including JAUNT. Please ensure you have data for all transit systems. We have four total bus systems and they are mostly to serve seniors. Many residents do not use the buses; it is very rural.
42.	The Virginia Breeze is a new service connecting Danville to DC through Farmville and Richmond. Group 3 inquired about the location of the stops.

Safety: Vehicle Crashes

43.	VDOT’s Potential for Safety Improvement (PSI) rating method removes drug or alcohol related incidents, so these incidents do not influence whether an area is shown to have PSI. How to account for distracted drivers?
44.	Please include safety hotspot or needs for additional safety studies.
45.	In some areas with a lot of fatalities and injuries, improvements have been made (e.g. New roundabout and other improvements at 5th Street at Federal Street in Lynchburg). The maps do not show where improvements have been made or how they have improved safety.

	Comment
46.	Can we think of an intersection that would have a safety issue but wouldn't be on the PSI list? If there's a systemic issue that could apply, how is this counteracted?
47.	The interchange of US 29, US 460, and US 501 is a top safety concern. The distance from the traffic signal to the ramp is only 500 feet, and the Concord Turnpike off-ramp is confusing. The traffic on US 460 has increased in recent years. US 501 to the Bypass needs to be limited access.
48.	Amelon Road (Rt 130) at the industrial park is a top safety concern. The speed limit abruptly decreases from 55 to 35 and there is no traffic signal. This area has poor lighting.
49.	There are three intersections along US 460 in Bedford County with safety issues: Route 811, Turkey Foot Road, and Wildwood Road. The 460 Corridor needs to eliminate crossovers and limited access improvements are needed.
50.	An intersection improvement is needed to address safety issues at US 29 at Lawyers Road and English Tavern Road.
51.	Route 130 in Amherst County has a lack of sight distance.
52.	The intersection of US 501 VA-24 in Rustburg needs access management.
53.	There are safety issues and geometric challenges for trucks traveling through the Town of Kenbridge and Town of Victoria.
54.	US 460 East near Farmville, near the bypass - There is bad geometry and narrow crossovers in this area. Trucks create problems and merging traffic that can't get out to go westbound.
55.	There are safety issues at two intersections along Route 15 in Buckingham County - at Route 20 and at Route 617.
56.	Amish communities with horse drawn carriages are starting to emerge near Route 60, especially near Route 24 in Buckingham County and near the Cumberland County line. This is also occurring in Charlotte County. There are signage needs for the buggies.
57.	Safely accommodating bicycling and non-motorized modes is an issue in the overall region. The High Bridge Trail and Twin Lakes State Park have increased bicycling activity. Commonwealth Regional Council has a bicycle plan that is around 10 years old and needs to be updated. The major problem in this area is there are no shoulders to ride on. Recreational bicyclists interact with logging trucks and other large vehicles.

	Comment
Economic Development: Urban Development Areas and VEDP Business Ready Sites	
58.	UDAs should be the focus of needs related to economic development, more so than the VEDP sites. Localities in the Central Virginia PDC area have adopted UDAs into their comprehensive plans. A shovel-ready VEDP site may not be included in a locality's comprehensive plan.
59.	Consider including Tier 3 areas (sites that are zoned). By having the zoning in place, the locality has a level of commitment to the site that is equivalent to the UDAs. Tier 3 sites should be included in VTrans for the rural areas. Local governments often have to lead on infrastructure investments.
60.	Some of the data and tiering for the VEDP sites looks to be out-of-date. The localities need to re-evaluate and update the list of sites so that it is accurate.
61.	Liberty University is concerned their needs are being left out in the Industrial and Economic Development Areas (IEDAs) as proposed.
62.	Group 2 identified several other sites of interest OIPI should consider including in the list of I/EDAs: <ul style="list-style-type: none"> • Montvale Tank Farm in Bedford County • Town of Altavista is expanding boundary of the town to incorporate approx. 55 acres as an industrial park. This area is a UDA. • A private business park at US 460 and VA-609 in Appomattox County is being developed. • Thomasville old furniture factory in the Town of Appomattox • Tyreeanna on the south side of US 460 from US 29 to the Bypass • Cheese Creek Development Site at Wiggington Road (Ivy Creek)
63.	Group 3 identified several potential new UDAs: <ul style="list-style-type: none"> • Town of Farmville • Route 15 Strategic Growth Area in Prince Edward County • US 460 East Strategic Growth Area in Prince Edward County • Route 45 Strategic Growth Area in Cumberland County (Town of Farmville) • US 460 West Strategic Growth Area in Prince Edward County • Route 15 Growth Corridor in Buckingham County • Town of Keysville Opportunity Zone • North of the Town of Keysville to US 360
64.	Virginia Heartland Park in Charlotte County - needs to be Tiered by VEDP
65.	Consider adding US 360 as an Emerging CoSS in the North/South Corridor

5 APPENDIX: COMMENTS FROM PLENARY PRESENTATION, BREAKOUTS, AND WORKSHOP HANDOUTS

i. COMMENTS DURING PLENARY PRESENTATION

The following section summarizes questions and comments about the topics covered during the plenary presentation by Jitender Ramchandani. Questions from participants are shown in italics, followed by brief responses from the plenary speaker.

VTRANS NEEDS ASSESSMENT PROCESS

- When asked whether VTrans should focus on an acute problem affecting only a few people or a moderate problem affecting a lot of people, most workshop participants initially raised their hand for a more moderate problem affecting many people. However, some participants noted that acute problem should not be ignored.

CONGESTION MEASURES

Percent Person Miles Traveled in Excessively Congested Conditions (PECC)

- No comments during the plenary presentation

Travel Time Index (TTI)

- No comments during the plenary presentation

RELIABILITY MEASURES

Unreliable Delay (UD)

- No comments during the plenary presentation

Buffer Time Index (BTI)

- No comments during the plenary presentation

PASSENGER RAIL

Amtrak Station On-Time Performance

- No comments during the plenary presentation

Virginia Railway Express On-Time Performance

- No comments during the plenary presentation

ACCESSIBILITY TO ACTIVITY CENTERS MEASURES

Transit Access Deficit to Knowledge-Based and Local-Serving Activity Centers

- *How are activity centers defined? Is there a specific number of workers that defines an activity center?* Activity Centers are generally defined as concentrated areas of employment and activity that generates travel demand. There is no single definition, as

the number of worker is relative to the region. It was also noted that Activity Centers were defined for each region as part of the previous VTrans2040 effort and the information presented today includes the Activity Centers carried forward from the previous plan and characterized on the maps by predominant type of employment (e.g. knowledge-based, local-serving and freight-dependent).

MULTIMODAL ACCESSIBILITY MEASURES

Travel Options: Disadvantaged Population Beyond ¼ Mile Access to Fixed Route Transit

- *Timberlake Road is served by a number of GLTC routes, but it is shown as dark blue. Why? This area may have a bus line running through it, but no bus stop within a quarter mile of the block group centroid.*

Potential measures for non-motorized accessibility analyses (not mapped yet)

- No comments during the plenary presentation

SAFETY MEASURES

Vehicle Crashes

- No comments during the plenary presentation

ECONOMIC DEVELOPMENT MEASURES

Urban Development Areas and VEDP Business Ready Sites

- A participant commented that the Commonwealth's approach to attracting economic development seems to be based on concentration of growth. The participant noted that the supply chain type of distribution depends upon having an efficient transportation system and a more dispersed network of warehouses and distribution centers.
 - The participant noted that sites identified on the IEDA map are primarily on the I-95, I-85, and I-64 corridors. In the rural counties, we would like to have more sites identified in our counties, but there are no transportation facilities capable of accommodating major truck traffic to access possible sites. We need transportation facilities that can accommodate major truck traffic in order to stimulate economic development. But the "model" you're using to determine where the transportation investments should go depends on the concentration of activity. But there's no concentration there, and there are no transportation facilities that can accommodate truck traffic to access.
- A participant noted that half of the Central Virginia MPO's UDAs are not in areas of concentrated growth. They are in former industrial parks or places where there is nothing today, but they are places where localities want to focus growth. These areas do not score well in SMART SCALE.

ii. BREAKOUT SESSION COMMENTS

The following synthesis reflects input from all the breakout groups. Participants were asked to reflect broadly upon the issues addressed by the performance measures (i.e., congestion, reliability, passenger rail on-time performance, accessibility to activity centers, travel

options for disadvantaged populations, safety, and economic development. They were also asked for input on the regional applicability of each measure.

Facilitators and scribes assigned to each group recorded the input by writing notes on a flip chart and on a laptop. For comments with geographic specificity, facilitators and/ or group members placed numbered stick-on dots onto a poster-sized base map and noted the meaning of the numbered dot on the flip chart.

Participants were invited to jot down additional notes on the Comment Form and return it to a facilitator at the end of the meeting, or to fill it out later and email their responses to OIPI staff. A summary of input from the written Comment Forms is included at the end of this section

GROUP 1 COMMENTS

Breakout Group 1 Marked Up Map



Congestion

PECC:

- US 460 and US 501 are limited access highways, but the maps do not show them as limited access. These highways are congested, but they are not shown on the map. This map and this definition of congestion do not reflect congested conditions in Lynchburg.

- In Map 1-CU, the part of the US 29 Bypass shown in red does not make sense - this segment is not congested. It may show up as red because traffic must slow down to get onto the ramp. Another reason might be because there was a construction project in 2018.

TTI:

- Map 2 is showing congestion on US 460 and Village Hwy (VA-24) between Appomattox (Dot 1) and Rustburg (Dot 2), which is surprising. Please double check this. The high school may be a contributing factor.
- Only a portion of Timberlake Rd (US 460 Business) (Dot 3) is showing up on the map. Participants expected more of it would show up as congested.
- There is a congestion issue at the intersection of US 221 and US 501 (Dot 4), but this area is not showing up on the map. An intersection improvement project here will receive funding from the most recent round of SMART SCALE.

Reliability

UD:

- It would be helpful to have the methodology for this measure. Reliability is relative to other areas.
- From first glance, the map seems to be a good representation.

BTI:

- At the intersection of US 221 and US 501 (Dot 4), US 501 northbound is not showing up, but it has reliability issues.
- Please add the ramps for this measure.
- The BTI map shows portions of the Blue Ridge Parkway as unreliable. The concept of reliability is not applicable to the Blue Ridge Parkway.

Accessibility to Activity Centers

The group discussed the activity centers. They questioned how the activity centers were defined and how they would be used. The group indicated many of the activity centers on the map did not accurately reflect the true centers of activity, especially those within the City of Lynchburg.

- Big Island (Dot 5) is not a local service activity center anymore. It is more of a freight-dependent activity center.
- Brookneal (Dot 6) is missing from the map and should be added as an activity center.
- Farmville (Dot 7) may need to be included. It is an isolated activity center.
- The Dearington activity center point should be moved to the hospital and health centers.

- The map shows a medium transit deficit at Liberty University, but there is no transit deficit within the campus nor in the immediate surrounding area. There is a difference between accessibility within an activity center and accessibility to an activity center.
- The Forest activity center is an industrial park.
- The Lynchpin Industrial Park has a lot of traffic and train service and should be added.
- Consider adding KDC off of Florida Avenue.
- Some of the activity centers may not have transit service and may not need transit service. Some activity centers have an aging/elderly population who may become more transit dependent.
- Should MPOs identify the activity centers if OIPI provides criteria? The Central Virginia MPO offered to do so if actual placement of the points matters.

Travel Options for Disadvantaged Populations

The maps seem a little odd or misleading at the local level. It would be helpful to show park-and-ride locations and other types of service (e.g. van-sharing) on these maps. Please clarify what the limitations of the data are. The Central Virginia PDC offered to provide more recent data on transit routes and stops.

The maps appear to be looking at the central geographic location of census tracts, not population centers. Some of the areas shown as disadvantaged actually have transit.

Location-specific comments:

- GLTC Route #5 goes into the Old Town Madison Heights area (**Dot 8**) but there is limited transit service here (only runs once every two hours). This area is not shown on the maps as disadvantaged, but it is, and it should be added. It is a geographically challenging (hilly) area with an urbanized pocket, and it has the highest percentage of low-income residents and high unemployment.
- Altavista (**Dot 9**) and the Town of Amherst (**Dot 10**) need transit connector service to Lynchburg. Please make sure the Town of Amherst is an activity center on the maps.

Some of the areas may not need fixed route service. Park-and-ride and van-sharing may be more appropriate.

Questions:

- How will VTrans address areas that are “not viable” for fixed route service? Do these areas have zero transit service needs?

Passenger Rail On-Time Performance

General comments about Passenger Rail:

- The Lynchburg region is served by the Northeast Regional and Crescent. The Northeast Regional is more reliable. The Crescent is not nearly as reliable.
- The NCDOT Rail Plan proposes service from Charlotte to Danville to Lynchburg.
- The business community would like to see earlier train arrival times to D.C. and improved air service from Lynchburg to Dulles.
- The Virginia Breeze intercity bus service is in the works.

Safety

General comments about Safety:

- There could be a safety issue where the green dots are concentrated.
- In SMART SCALE, fatalities produce a very high score even when the fatality had nothing to do with the roadway, like drunk pedestrians.
- VDOT's Potential for Safety Improvement (PSI) rating method removes drug or alcohol related incidents, so these incidents do not influence whether an area is shown to have PSI. How to account for distracted drivers?
- The purpose of the PSI lists is to identify areas that need a closer look. Include more information with the PSI to better identify the safety issues and possible causes.
- VDOT noted they have seen that when you straighten a road or put wider lanes, people feel like they can go faster. VDOT looks at traffic calming measures.
- **Please include safety hotspot or needs for additional safety studies.**
- In some areas with a lot of fatalities and injuries, improvements have been made (e.g. New roundabout and other improvements at 5th Street at Federal Street in Lynchburg). The maps do not show where improvements have been made or how they have improved safety.

General questions

- *Can we think of an intersection that would have a safety issue but wouldn't be on the PSI list? If there's a systemic issue that could apply, how is this counteracted?*

Economic Development

General comments about Economic Development:

- **UDAs should be the focus of needs related to economic development, more so than the VEDP sites.** Localities in the Central Virginia PDC area have adopted UDAs into their comprehensive plans. A shovel-ready VEDP site may not be included in a locality's comprehensive plan.
- **Consider including Tier 3 areas (sites that are zoned).** By having the zoning in place, the locality has a level of commitment to the site that is equivalent to the UDAs.

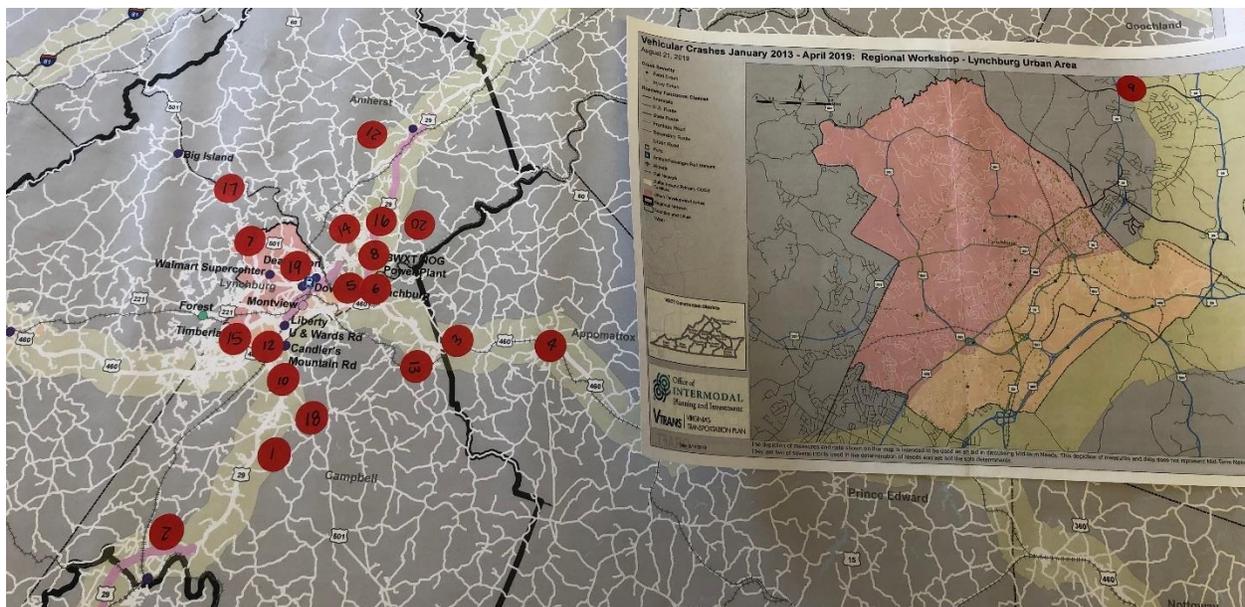
- Some of the data and tiering looks to be out-of-date. The localities need to re-evaluate and update the list of sites so that it is accurate.
- Concern that Liberty University’s needs are being left out. Liberty University is a private university and a large trip generator. We do things in our own interest but want to be a good neighbor to Lynchburg. There is a need for improved communication between LU and localities.

General questions:

- Do you cover UDAs that are not in urban areas? “Urban” doesn’t accurately describe these areas in non-urban areas. OIPI clarified that localities can use any term for the area as long as the code reference is included in the comprehensive plan.

GROUP 2 COMMENTS

Breakout Group 2 Marked Up Map



Economic Development

VEDP Industrial and Economic Development Areas:

- The Appomattox site is a Tier 2.5
- Seneca Park (Dot 1) in Campbell County is listed as Tier 2 but should be Tier 3. This area is a UDA.

The group discussed other sites of interest to consider including in the list of I/EDAs:

- Montvale Tank Farm (over 100 acres) in Bedford County
- The Town of Altavista is zoning about 20% of the town (approx. 55 acres) as an industrial park (Dot 2) This area is a UDA.

- A private business park (Dot 3) at US 460 and VA-609 in Appomattox County is being developed. There is a lot of development happening between VA-609 and the Campbell County line.
- Thomasville old furniture factory (Dot 4) in the Town of Appomattox
- Tyreeanna (Dot 5) on the south side of US 460 from US 29 to the Bypass - The City of Lynchburg is conducting a development study here.
- Cheese Creek Development Site (Dot 7) at Wiggington Road (Ivy Creek)

The group also discussed the two separate commercial service airports (Lynchburg and Roanoke) serving the area, which are both struggling as they compete against one another. In hindsight, one more centrally located larger airport (in Bedford) to serve the region, had it happened years ago, would have been the better option.

Safety

Top Safety Concerns:

- Interchange of US 29, US 460, and US 501 (Dot 6) - The distance from the traffic signal to the ramp is only 500 feet, and the Concord Turnpike off-ramp is confusing. The traffic on US 460 has increased in recent years. To stay on the US 29 Business, drivers must make multiple turns - this is confusing and unsafe. US 501 to the Bypass needs to be limited access. Amelon Road (Rt 130) at the industrial park (Dots 8 and 9) - The speed limit abruptly decreases from 55 to 35 and there is no traffic signal. This area has poor lighting. There are three intersections along US 460 in Bedford County with safety issues: Route 811, Turkey Foot Road, and Wildwood Road. The 460 Corridor needs to eliminate crossovers and limited access improvements are needed.
- US 29 at Lawyers Road and English Tavern Road (Dot 10) - An intersection improvement is needed here. Drive times have increased, and there are 400 new housing units proposed.
- Route 130 in Amherst County has a lack of sight distance.
- The intersection of US 501 VA-24 in Rustburg needs access management.

The Top 100 PSI Lists generally capture safety hot spots in Bedford County.

Congestion

PECC:

- US 29 Business at Madison Heights is a top congestion issue in Amherst County. The congestion shown on US 29 Business on Map 1-C is accurate and could be extended further north.
- The US 29 Bypass crossing over the river (Dot 11) is showing as red in Map 1-C, but there is no congestion here. This is a data anomaly.

- US 460 Westbound between Candler's Mountain Road and Greenview Drive (**Dot 12**) has congestion, but the maps do not show it. Traffic on the Candler's Mountain Rd off ramp backs up onto 460 Westbound.
- School traffic at every school during drop-off and pick-up times causes congestion on main and secondary roads. The Town of Appomattox is surrounded by schools and it experiences gridlocked traffic for 45 minutes in the morning and afternoon.

TTI:

- Route 24 from Appomattox to Rustburg (**Dot 13**) is showing as having congestion (green), but there is no congestion here. There was roundabout construction that could have caused the map to light up.
- US 29 Business to Lakeview Drive (**Dot 14**) is congested during peak hours but isn't showing up on the map.
- The intersection of US 501 at US 221 is congested, but it does not show up on the congestion maps. US 221 is congested from US 501 to Route 663.
- The area of Laxton Road to Timberlake Road to Greenview Drive (**Dot 15**) is a congested area, but it is not shown on the congestion maps.

Reliability

UD:

- The reliability maps show US 29 Bypass through Amherst County (**Dot 16**) and in the southern part of Campbell County as unreliable, but it is one of the most reliable roads in the region. When accidents do occur, it can take several hours to clear up and there is nowhere to get off. These are rare occurrences.
- Maps 3A and 3B show US 29 Business (Lynchburg Expressway) in Amherst County and City of Lynchburg as unreliable (blue). There are reliability issues here, and the map accurately reflects them.

BTI:

- Map 4 shows US 501 from the City of Lynchburg to Big Island (**Dot 17**) as unreliable. There is a lot of truck traffic on this road, and you have to add in buffer times when traveling.
- Map 4 shows Campbell County Route 24 (**Dot 18**) as unreliable, but there are no reliability issues here.
- Map 4 shows the intersection of US 460 at Route 24 (at the Appomattox and Campbell County border) as unreliable, but there are no reliability issues here.
- The Blue Ridge Parkway is shown in blue. This could be deleted.

Accessibility to Activity Centers

In general, the activity centers are hard to understand. The group questioned how the activity centers are defined. There are other Walmart Supercenters outside of Lynchburg that are not showing up. Localities have Points of Interest layers that could be used to identify the activity centers.

The Dearington activity center point (Dot 19) should be moved to the hospital. The hospital is a major employer in the area.

Travel Options for Disadvantaged Populations

Appomattox County and Bedford County do not have any transit service, but the map is showing fixed route service here. Why is it shown?

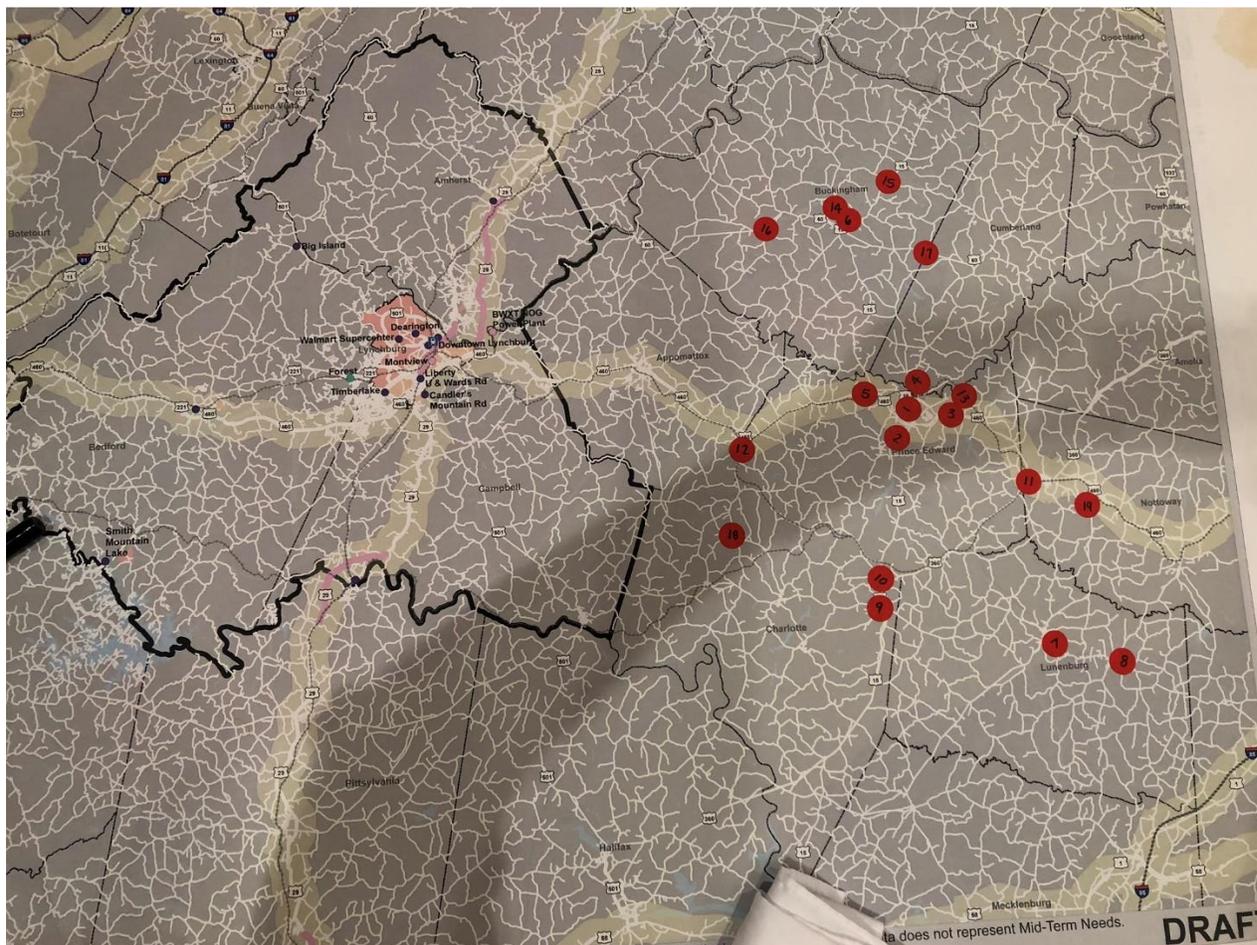
This measure is odd and misleading in this area, especially in Amherst and Bedford counties. There are trailer parks and people who are disadvantaged living next door to high-income households. Some areas are national forest with no people living there - how can they be disadvantaged if no one lives there?

The area around Dot 20 is not disadvantaged, but it is shown in Map 7. It should not be shown.

The area around Dot 21 is one of the most low-income areas in Amherst County, but it is not shown in blue. It should be shown.

GROUP 3 COMMENTS

Breakout Group 3 Marked Up Map



Economic Development

Potential New UDAs:

- Town of Farmville (Dot 1)
- Route 15 Strategic Growth Area (Dot 2) in Prince Edward County
- US 460 East Strategic Growth Area (Dot 3) in Prince Edward County
- Route 45 Strategic Growth Area (Dot 4) in Cumberland County (Town of Farmville)
- US 460 West Strategic Growth Area (Dot 5) in Prince Edward County
- Route 15 Growth Corridor (Dot 6) in Buckingham County - there is a code reference in the comprehensive plan
- Town of Keysville Opportunity Zone (Dot 9)
- North of the Town of Keysville to US 360 (Dot 10)

I/EDAs:

- Virginia Heartland Park in Charlotte County - needs tiering

- Tier 3 sites should be included in VTrans for the rural areas. Local governments often have to lead on infrastructure investments. You can have the infrastructure in place already, but we have issues with zoning. (Private development has a lot of resistance though if zoning is not already in place.)
- The labor market dictates what industry we can bring into our area.
- Our area would like to add new knowledge centers. A lot of people are commuting from metro Richmond to our area for work at the large facilities. Consider adding US 360 (Dot 11) as the North/South Corridor - Emerging CoSS for consideration.

Questions:

- *How are the CoSS chosen?*
- *How do we designate a UDA? Do we need to reference the code section in the comprehensive plan?*
- *If Farmville is a UDA, how do we designate areas around the town?*
- *When you look at UDAs, is it a combination of commercial and residential?*

Congestion

TTI:

- The green line on US 460 between Appomattox and Lynchburg is slow traffic going into Lynchburg in the morning and leaving Lynchburg in the evening. This is very consistent in peak periods. This confirms the commuter flows between the Towns and the Regional Networks.
- The green lines and spots in the Town of Farmville reflect congestion issues due to shifts in seasonality and when the university comes back in session.

Reliability

BTI:

- The BTI map shows Old US 460 near Pamplin (Dot 9) in Prince Edward County as blue. Participants did not understand why this was showing up.
- Route 20 in northern Buckingham County is shown in blue. There was a construction project here in 2018.
- There is a blue dot west of Farmville. Please review this dot.

Safety

The group identified safety issues:

- There are safety issues and geometric challenges for trucks traveling through the Town of Kenbridge (Dot 7) and Town of Victoria (Dot 8)
- US 460 East (Dot 13) near Farmville, near the bypass - There is bad geometry and narrow crossovers in this area. Trucks create problems and merging traffic

that can't get out to go westbound. When an incident occurs on US 460, it pulls traffic and trucks onto local streets through Farmville.

- Route 20 and Route 15 in Buckingham County (Dot 14)
- Route 15 and Route 617 in Buckingham County (Dot 15)
- Amish communities with horse drawn carriages are starting to emerge near Route 60, especially near Route 24 in Buckingham County (Dot 16) and near the Cumberland County line (Dot 17). This is also occurring in Charlotte County (Dot 18). There are signage needs for the buggies.

Safely accommodating bicycling and non-motorized modes is an issue in the overall region. The High Bridge Trail and Twin Lakes State Park have increased bicycling activity. CRC has a bicycle plan that is around 10 years old and needs to be updated. The major problem in this area is there are no shoulders to ride on. Recreational bicyclists interact with logging trucks and other large vehicles.

Passenger Rail On-Time Performance

The group discussed both passenger and freight rail.

- Intermodal freight is a large economic development opportunity. The Buckingham Branch railroad company is considering developing an intermodal center at the Heartland Industrial Park in Charlotte County, which would take trucks off the roads. The Town of Crewe (Dot 19) is an intermodal truck point.
- The Trans-Dominion Express (TDX) from Bristol to Richmond is an old concept. It is not on DRPT's radar right now. The Roanoke Service may be filling this gap.
- The group suggested OIPI show the short lines on the maps.

Travel Options for Disadvantaged Populations

Farmville is served by multiple transit systems including JAUNT. Please ensure you have data for all transit systems. We have four total bus systems and they are mostly to serve seniors. Many residents do not use the buses; it is very rural.

The Virginia Breeze is a new service connecting Danville to DC through Farmville and Richmond. The group inquired about the location of the stops.

iii. COMMENT SHEET INPUT

The following section lists the written input from participants who chose to fill out the printed comment sheet in their meeting packets. Key points and concepts from this input are reflected in Table 2 (Synthesis of Comments). Some participants planned to send comments to OIPI staff after the meeting; input from these post-meeting messages may not be captured in this meeting summary, but OIPI is considering all continued input during the development of the needs assessment.

Only one comment sheet was submitted at the Lynchburg Area workshop. Many portions of the comment sheet were left blank. Only those sections that had comments are included below.

Accessibility to Activity Centers

Activity Centers should be defined by the number of employees and the needs of those employees, in addition to other factors. Also, need to define the centers outside of MPOs too.

Economic Development

Freight rail needs should also be a focus. Short lines, like Buckingham Branch Railroad (BBRR), provide last mile connections that can help industries that may not otherwise be compatible with freight rail. More opportunity for taking trucks off the road. And BBRR is interested in an intermodal yard at Heartland Business Park near Lynchburg - could help with truck congestion on US 460, I-64, and I-81 coming from the port.